



# ARCHITECTURAL GUIDELINES

## Southfork – Stage 3A *TAMANI COMMUNITIES*

### **RSL Lots** **Front Drive Attached Garage** **R-1E**

November 16, 2010

# TAMANI COMMUNITIES

## Southfork - Leduc

### Front Drive Attached Garage R1-E (RSL) Lots – Stage 3A Architectural Guidelines

#### DEVELOPER

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#### ARCHITECTURAL DESIGN CONSULTANT

Horizon Designs Ltd.  
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May Molofy, Architectural Design Consultant

#### SURVEYOR

No mandated Surveyor. Any surveyor used must have ALS designation  
Under Review

#### ENGINEER

Focus Engineering  
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# Southfork - Leduc

## ARCHITECTURAL GUIDELINES

### FRONT DRIVE ATTACHED GARAGE RE-1 (RSL) LOTS

The information contained herein is provided as a guide to be used by builders/homeowners only. The Developer and its designated Architectural Design Consultant (ADC) make no warranties or representation as to the accuracy or completeness of these guidelines. Builders shall be fully and solely responsible for all designs, which follow these guidelines, and for the conformance to the appropriate building codes. These guidelines may be altered, amended, or varied by The Developer in its sole and absolute discretion.

#### OBJECTIVE

The Architectural Guideline is the mechanism which contributes to a high level of quality design in the community, thereby achieving the highest possible standard of visual appeal, benefiting the builder initially and the residents ultimately.

House plans will be reviewed in terms of the adherence to the design guidelines as detailed in this brochure. Certain modifications to house plans, elevations, and / or specifications may be requested where it is felt modifications are required to take advantage of unique lot characteristics or to conform to the overall objectives of the guidelines and the development on a lot by lot basis.

Applicants may provide alternative details to those outlined in these guidelines; however, they must demonstrate that conformity to the overall objectives for the quality of the area is being maintained. The acceptability of such alternatives is solely at the discretion of The Architectural Control Panel.

#### CONCEPT

Southfork is a master planned community located in south eastern Leduc, Alberta just off the Queen Elizabeth II Highway. Southfork boasts single detached, single rear lane and semi-detached front drive housing and Stage 3 will add an additional 255 lots to this area. Southfork boasts small town charm with access to City of Edmonton conveniences only a 15 minute drive away as well just as a short drive to the International Airport. Residents of Southfork have easy access to Leduc's established core of schools, emergency services, banking, shopping, theatre, and state of the art multipurpose recreational facility. Within the Leduc area there is also the William F. Lede Regional Park and Telford Lake where families can enjoy canoeing and kayaking.

Each neighborhood will employ architectural guidelines to ensure that the COMTEMPORARY HERTITAGE vision for this community is emulated. The goal in Southfork is to develop homes with architecture that draws on traditional architectural elements while addressing the needs of contemporary living and modern materials.

#### DISPUTES

Individual concerns will be presented by ADC to the The Architectural Control Panel – Developer to be adjudicated and their decision will be final.

# ARCHITECTURAL GUIDELINES

## 1) GENERAL DESIGN

Builders will be encouraged to utilize the following guidelines in order to achieve and maintain comprehensive image throughout the community. Emphasis will be concentrated on trying to create a strong “curb appeal” to each home through attention to detail on the front entry.

## 2) ELEVATIONS/MODELS - Repetition

- a) The same model/elevation must be separated by 2 lots on the same side of the street (XAOX) and it will not be allowed directly across the street.
- b) This may be altered at the ADC discretion if it can be shown that the two houses in question are located so as not to be visible together from any given angle. While similar house plans and elevations are inevitable, it is possible to vary the design by changing the roof line design, and size and location of windows and doors etc. It is felt that adjacent houses can be altered to make it suitable.
- c) Revised elevations will be requested so that an accurate set of files can be kept for each lot.

## 3) COLORS

- a) Colors will be approved on a lot by lot basis. Dark Bold contrasting colors are the central theme to emulate the Contemporary Heritage Home style.
- b) **SECONDARY COLORS** - each home is permitted to have one secondary color that is complementary to the main body color of the home. This color may be used on accent materials such as shakes or gable treatment. All secondary colors will be approved at the discretion of the ADC.
- c) **COLOR REPETITION**- cannot be repeated within 2 lots on the same side of the street (XAOX). The ADC has the authority to make changes in a color scheme, so that it blends with the adjacent homes. The ADC will make the final decision on the color of the houses. INTENSE SHADES will be reviewed on a lot by lot basis at the sole discretion of the ADC.
- d) **CORNERS** – front must be wide white  
Rear corners on lots backing onto chain link fencing must be white as well
- e) **FRONT ENTRY DOOR** – must be painted in a contrasting deep/vibrant color or white
- f) **TRIMS** – must be white, including, window detail, shadow boards, porch columns, railing to punctuate contrasting darker siding colors.
- g) **FASCIA/SOFFIT/EAVES/GUTTERS** – mandatory color to be White.
- h) **STONE OR BRICK** selection cannot be repeated on adjacent lot (XOX)
- i) **GARAGE OVERHEAD DOORS** must be white on white, the door the same color as the predominate wall color with white trim or the door white with the trim the same color as the predominant wall or trim color.

#### 4) MINIMUM HOUSE SIZES

- a) Square Footage's as per City of Leduc R-1E Residential land use bylaw
  - Bungalows and Bi-Levels 967.7 sq ft (not including bonus room)
  - 1 ½ Storey 1,290.3 sq ft (not including bonus room)
  - 2 Storey 1,300 sq ft

Massing of the pocket is also required. IF the full pocket is not used the deviation cannot be more than the house being within **4-feet** of the pocket width and the garage being within **2-feet** of the pocket width. An offset to the garage may be required in order to obtain sufficient massing.

- b) Minimum house sizes are established reviewed and may be amended from time to time by the Architectural Control Panel. Standards and approvals recognize the specific size, nature, and location of a site and the desire to avoid extremes.

Consideration is also given to optimizing sight lines and creating acceptable size transitions between adjacent homes and areas. Excessive total building height differences between adjacent dwellings will be reviewed on a lot by lot basis. Revisions may be required in order to reduce differences that are greater than 6-feet.

- c) All homes in this phase require a double front attached garage.

#### 5) CORNER LOTS

- a) Special attention must be paid to side elevations and side yard setbacks on all corner lots.
- b) Side elevations facing a street are to be consistent in terms of design and detailing with front elevations i.e.) window & door openings, roof lines etc. with visibility reviewed on a lot by lot basis.
- c) Two storey plans will be allowed on all corner lots.
- d) Side drive garages will be reviewed on a lot by lot basis.

#### 6) REAR ELEVATIONS, onto park & walk way

Any lot with high visibility i.e.) backing onto main roadway, park, pond, walk way, greenbelt or school site requires additional detailing.

Rear Corners visible from roadway, park etc, must be white matching front elevation corner detail in color however may be 3" vs the wide required for the front.

Where rear fence is wood screen – second storey window & door openings require detailing  
Where rear fence is chain link – ALL window & door openings require detailing.

#### 7) SETBACKS / PARGE LINE / RISERS

- a) **SETBACKS** - In addition to the City of Leduc's requirements for minimum setbacks, all houses should be built within the building envelope defined for each lot. However, builders may be required to adjust setback.

Houses should be placed perpendicular to the road, rather than angled on the lot, not parallel to one of the side property lines, whenever possible. The streetscape will flow more smoothly if the homes are positioned in a radial manner on curved roads. The line of houses on the street should follow the natural curve of the street.

- b) **PARGE LINE** -No more than of 24-inches of parge will be allowed on elevations visible ahead of the fence tie-back line including at entrances and any elevations visible from any main roadway.
- c) **RISERS AT ENTRANCE** - Due to City of Leduc Land Use By-law not allowing window wells, all measures are to be made to each individual house plan to keep risers at the entrance to a minimum.  
**No more than 4 exterior risers per set will be allowed at the front entrance, any further that may be required must be placed in the run of the walk or inside the entrance.**

**8) ELEVATION DETAILING** - The amount of stone, wood trim, crezone panels, muttin bars, shutters, porches, or louvers used to provide interest and a sense of individuality while maintaining continuity to the streetscape will be consistent with the typical streetscape throughout the neighbourhood.

- a) **FALSE FRONT** material will not be permitted unless the material utilized on the front elevation of the dwelling is either brick or stone complete with a minimum 24-inch wrap onto the side elevation.
- b) **EXTERIOR MATERIAL** - Vinyl siding or Hardie Board siding only (stucco exterior will not be permitted).
- c) **SECONDARY WALL MATERIAL** may consist of cedar shakes, composite shakes, vinyl shakes, smart panel, board or batten detailing, stone or brick.
- d) **WINDOW & DOOR OPENING DETAIL** - All front window and door openings require trim detail of full battens, top and bottom sills, or shutters.
- e) **ENTRANCE COLUMNS** - to be simple line detail, corbelling at top or bottom or both allowed, narrow corner detailing acceptable. No double columns or in-lays allowed.
- f) **FRONT ENTRY DOORS** - It is strongly suggested to use glass detail, however is not mandatory
- g) **BRICK OR STONE** - is not mandatory, however should it be used, it must be used in portions reflecting structural integrity. Where used must be minimum 3-feet in height on a wall surface, up to decking on veranda skirting. All stone or brick must wrap a minimum of 2-foot onto side elevations.
- h) **RAILINGS** - acceptable materials, aluminum or painted or stained wood (natural pressure treated will not be allowed).
- i) **WINDOW GRILLS OR MULLION** separation are optional, however if used must be used on all windows @ front elevation.
- j) **CORNERS @ front elevation must be WIDE**  
May be vinyl, wood, or smart board. No metal clad corners will be allowed.

## 9) ROOFS

- a) **ROOF MATERIAL** - shall consist of asphalt shingles.  
BP, Harmony - 35-year West Series or IKO, Cambridge - 30-year

- b) **ROOF COLOR** – mandatory color - black
- c) **ROOF LINES** - are to be varied along the street through the use of varied designs such as hip, gable, dutch gable and cottage designs.
- d) **ROOF PITCH** - All main roof slopes will have a minimum 6/12 pitch where possible. (1<sup>st</sup> storey, 2 storey, garage & entry)
 

Lower slopes allowed on verandas where roof line would interfere with 2<sup>nd</sup> storey windows.

Bungalows & Bi-levels to have steeper roof pitches in order to reduce the overall height difference with adjacent dwellings. Minimum 7/12 or steeper
- e) **OVERHANGS** - Soffit overhangs are to be a minimum of 18-inch except over a cantilever, bay or boxed out window where the minimum overhang required is a 6" configuration.
- f) **FASCIA** - shall be minimum 8-inches
- g) **CHIMNEY** – All chimney flue or stacks that may be required must be boxed in with the same finish as the main body of the home.

## 10) **GARAGES / DRIVEWAYS**

- a) **DRIVEWAY** – should be the full width of the garage itself, not the width of the garage door.
- b) **GARAGE** – to be double, front attached.
- c) **GARAGE OVERHEAD DOOR** – to be upgraded coach style.
- d) **DISTANCE FROM OVERHEAD DOOR TO SOFFIT/EAVE LINE** – cannot be more than 2-feet without the addition of detailing ie) wider trim detail @ overhead door, lateral detail @ soffit line, stone/brick, or in case of an open gable – shakes, craftsman detail, louvre, wide shadow board.
- e) **SIDEWALKS** – are to consist of the same material as the main part of the driveway and are to be poured concurrent with the driveway. Sidewalk stones are not permitted.

## 11) **LANDSCAPING -**

- a) **SECURITY DEPOSIT** – It is the builder’s responsibility to retain a landscape security deposit from The Homeowner, to be released upon approved final/landscape inspection by ADC unless the builder is providing a landscape package.
- b) **COMPLETION OF LANDSCAPING** - must be completed within one year of occupancy.
- c) **FRONT YARD LANDSCAPING** – to the fence tie backline is the responsibility of The Homeowner.
- d) **FRONT YARD LANDSCAPE REQUIREMENTS** – ahead of the fence tie-back line be completed with the following requirements;
  - i) A minimum of 6" topsoil & natural Sod to the fence tie-back line. No synthetic sod will be accepted.

- ii) Minimum of one (1) tree; Minimum tree size requirements at time of planting
  - 1.5" caliper if deciduous (leaf bearing) Caliper measurements are taken 12" up from ground cover.
  - 6' in height if coniferous (Spruce or Pine); Pyramid cedars and upright Junipers will not be accepted as coniferous trees.

iii) The Developer will supply one tree per lot (this tree can NOT be used as part of the minimum landscape requirement.)

iv) No FULL front yard, hard surface landscaping will be considered.

- e) **ACCESSORY BUILDINGS** – if constructed are to be of similar materials & colors as approved for the dwelling, where possible are to be placed out of view line from any commuter roadway, park, etc.

Lots backing onto the park, with Developer chain link fencing; sheds must be placed toward the rear of the house vs. toward the rear of the property.

## 12) FENCING

- a) It is the responsibility of the Builder to inform the Homeowner that it is their full responsibility to maintain any fence installed by the developer.
- b) Any interior fencing is to be overlap board wood screen, chain link, or wood screen, built as per diagrams provided by The Developer.
- c) Mandatory color – white throughout the neighborhood, including interior fence & tie-back fence.

## 13) RETAINING STRUCTURES

Builder/Homeowner is responsible for any retaining walls or wing walls that may be required as shown on final plot plan or otherwise.

## 14) DOWNSPOUTS

Must be directed away from the house, to streets or rear lanes.

Storm water must not be directed onto adjacent lots.

## 15.) SIGNAGE

Signage will be allowed on builder's lot only.

## 16) EXCAVATION MATERIAL

Builders must ensure that all excavation is kept within the confines of their lot. Any spillage on a road, sidewalk, or neighboring lot must be removed immediately or The Developer will arrange for its removal and invoice for expenses.

**Clean Up** - Builders/homeowners should encourage timely removal by all subtrades of litter on building sites. Failure to comply will result in a clean-up bill being charged to the lot. Supply of bins by the builder



is MANDATORY. Any general clean up of the subdivision implemented by The Developer can, and will be charged prated to all builders.

## 17) LOT GRADING

Lot grading is to follow the natural slope of the land form and is to be consistent with the Subdivision Grading Plan (which will be provided by The Developer). The minimum slope allowed is 2%. The Surveyor will prepare all plot plans. The staking out of the home will be jointly carried out by the builder and Surveyor. This procedure will facilitate establishing building elevations that are appropriate for the on-site grading conditions.

The Developer reserves the right to adjust the grading requirements between units after plans for affected homes have both been submitted. A coordinated grading review will be carried out by the Surveyor to ensure that the proposed final grades of a particular house do not adversely affect the adjacent house. The final grading requirements will be balanced by the Surveyor to the mutual benefit of both houses. It is the responsibility of the Purchaser, after final grading is complete, to provide to the Developer an approved Lot Grading Inspection from the City of Leduc that certifies compliance of the actual finished grades with the Approved Grading Plan.

Side-yard Grading: In addition to the critical grade control points at the corner pins of the lots, the grade elevations along the entire length of side property lines will also be important considerations.

All other guidelines contained herein are to be followed.

Conformity with these guidelines does not supersede the required approval process by The City of Leduc.

## 18) SECURITY DEPOSIT

A letter of credit deposit will be held by The Developer to cover all lots in this phase. Damages or infractions will be invoiced on a per lot basis by The Developer to The Builder for the following violations thereof, may retain the deposit or a portion:

### **Contravention of architectural objectives.**

#### **Damage to:**

Curb stop – water service valve  
Sidewalks, curbs and gutter  
Asphalt  
Boulevard landscaping and trees  
Light standards  
Fire hydrants

Catholic protection points  
Grading and drainage swales  
Any Developer provided improvement  
Fencing

#### **Security Deposit Return**

Exterior completed  
Rough grade completed  
Water valve exposed and marked

Sidewalks and curbs cleaned  
There will be a fee for re-inspection

## DAMAGES

Cost of repairs due to builder damages to existing features and utilities will be billed to The Builder or deducted from The Builder's security deposit and builder will be invoiced for damages beyond what the security deposit covers.

Builders are encouraged to offer their customers a choice of colors and materials on any lot prior to application. Once approval has been made, changes will be discouraged to limit conflict with neighbors.

Should changes be necessary, they must be approved in writing by the coordinator prior to proceeding.

## 19) ARCHITECTURAL APPROVAL PROCESS

- a) Before applying for a Development or Building Permit, the applicant shall submit the following package for review to Horizon Designs Ltd.
  - One complete set of house plans.
  - Plot plan, prepared by designated surveyor, showing lot house grades, and drainage pattern, floor and garage elevations and number of risers.
  - Horizon color application form complete with material, manufacturer, product names, & product codes.
- b) Horizon Designs Ltd. will review the plan and recommend approval, modification, or rejection of the application based on the adherence of the plans to these Guidelines within 7 business days from submission of complete application (circumstances permitting).

Once approved, Horizon Designs Ltd. will send the application review comments requesting revisions as required.

Revised plans & colors are required to be submitted in writing prior to final approval being granted.

After approval, the plans may not be altered without prior approval of Horizon Designs Ltd.

Approval of any and all house plans will be at the sole and unfettered discretion of the Developer.

- c) Should disputes arise, the Developer shall make the final decision on the acceptability of plans.
- d) A stake out date cannot be booked without final architectural approval.

## 20) IMPORTANT NOTES

- a) **CITY REGULATION** - Issuance of plot plan in no way absolves builders from compliance with all City and Provincial statutes
- b) **START** - Site work must NOT commence prior to Architectural Approval being granted and land agreements fully executed with The Developer.
- c) **INTERIM BUILDING REVIEW** - The Developer &/or the ADC may carry out an onsite review of the homes during construction. Periodic checks made to ensure conformance to approved grading plans and the Design Guidelines. Modifications may be requested related to actual site conditions.

d) **PRE-INSPECTION** - Builder must pre-inspect their lot for damages to services, walks or curbs and list any damages in writing to The Developer.

e) **ENGINEERING DRAWINGS** - Builders must check engineering drawings and the lot to assure that nothing obstructs driveways.

f) **DISCRETION** – Notwithstanding anything else set out in these guidelines, Tamani Communities and the ADC may apply their respective judgements when considering and approving anything regulated or controlled by these guidelines. In so doing, the Developer and its consultants may provide waivers of or relaxations to any matter set out in these guidelines in their sole and absolute unfettered discretion.

In case of disagreement, Tamani Communities will render a final decision

**No Right to Enforce** – Only the developers may enforce the guidelines, no purchaser of a lot in this area may enforce these guidelines.

**Right to Amend** – The Developer may from time to time amend these guidelines as it sees fit in its sole and absolute unfettered discretion.

## 21) FORMS

Following are attached:

Pre-construction lot inspection form

Fencing details

Current version of the following may be obtained from Horizon Designs upon request.

Color application form

Final inspection request

July 13, 2010, guidelines finalized.

Nov. 16, 2010, rear corners when onto park, roadway etc must be white

10-d – overhead door eave line detail

8-c, added brick/stone as a secondary material option

3-j, garage door color

8-e, entrance columns

# **PRE-CONSTRUCTION LOT INSPECTION REPORT**

## **DEVELOPER – TAMANI COMMUNITIES**

Date of Inspection: \_\_\_\_\_

Subdivision: - Southfork    Stage: 3A

Lot: \_\_\_\_\_    Block: \_\_\_\_\_    Plan: \_\_\_\_\_

Municipal Address: \_\_\_\_\_

Purchaser: \_\_\_\_\_

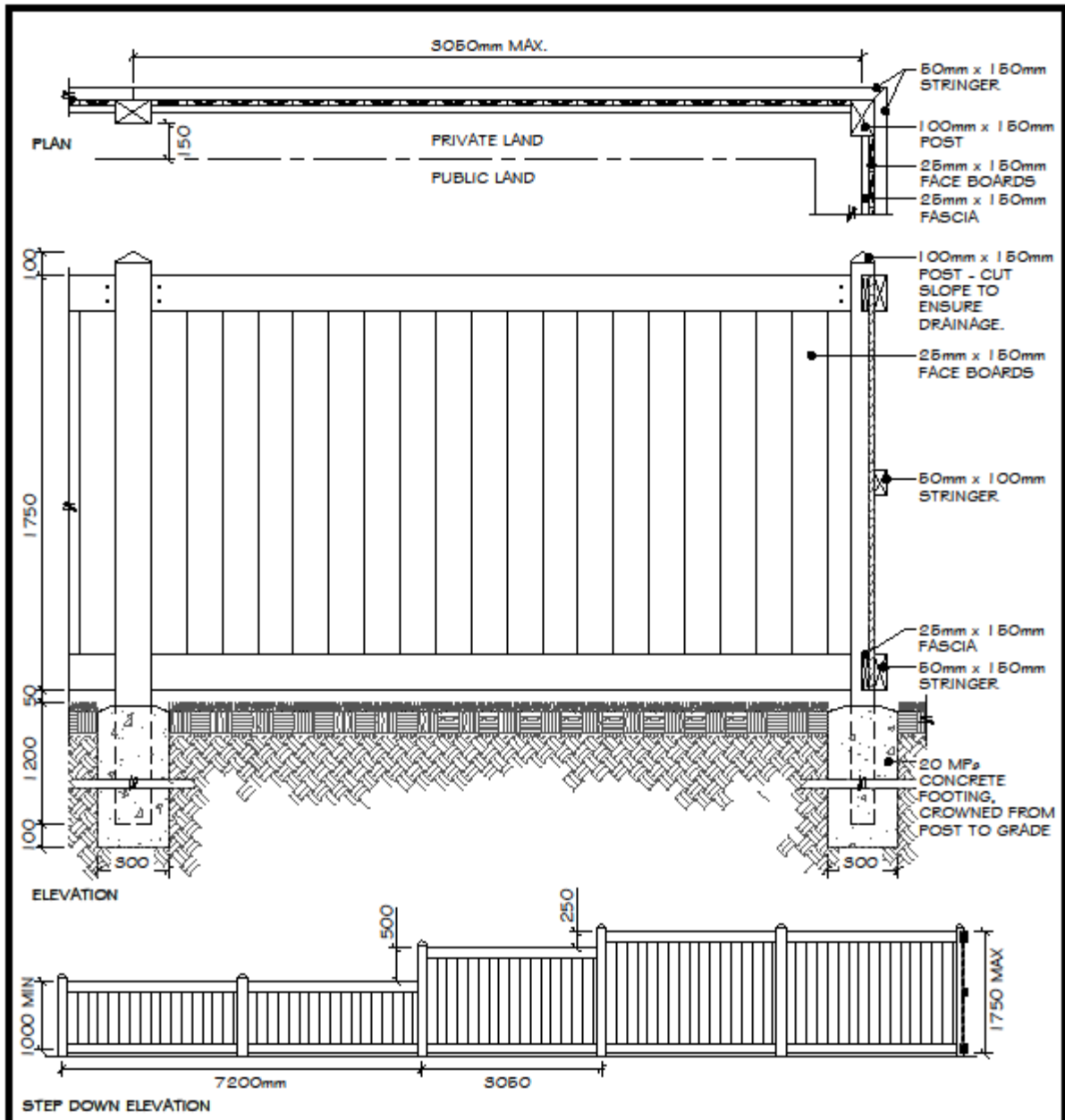
Item	Damaged		Description
	Yes	No	
Sidewalk			
Curb			
Driveway Apron			
Service Valve			
Swale			
Fence			
Other			

It is the Purchaser's responsibility to inspect all of the above items prior to stake-out. Snow, mud or other conditions do not release the Purchaser from this responsibility.


\_\_\_\_\_  
Signature

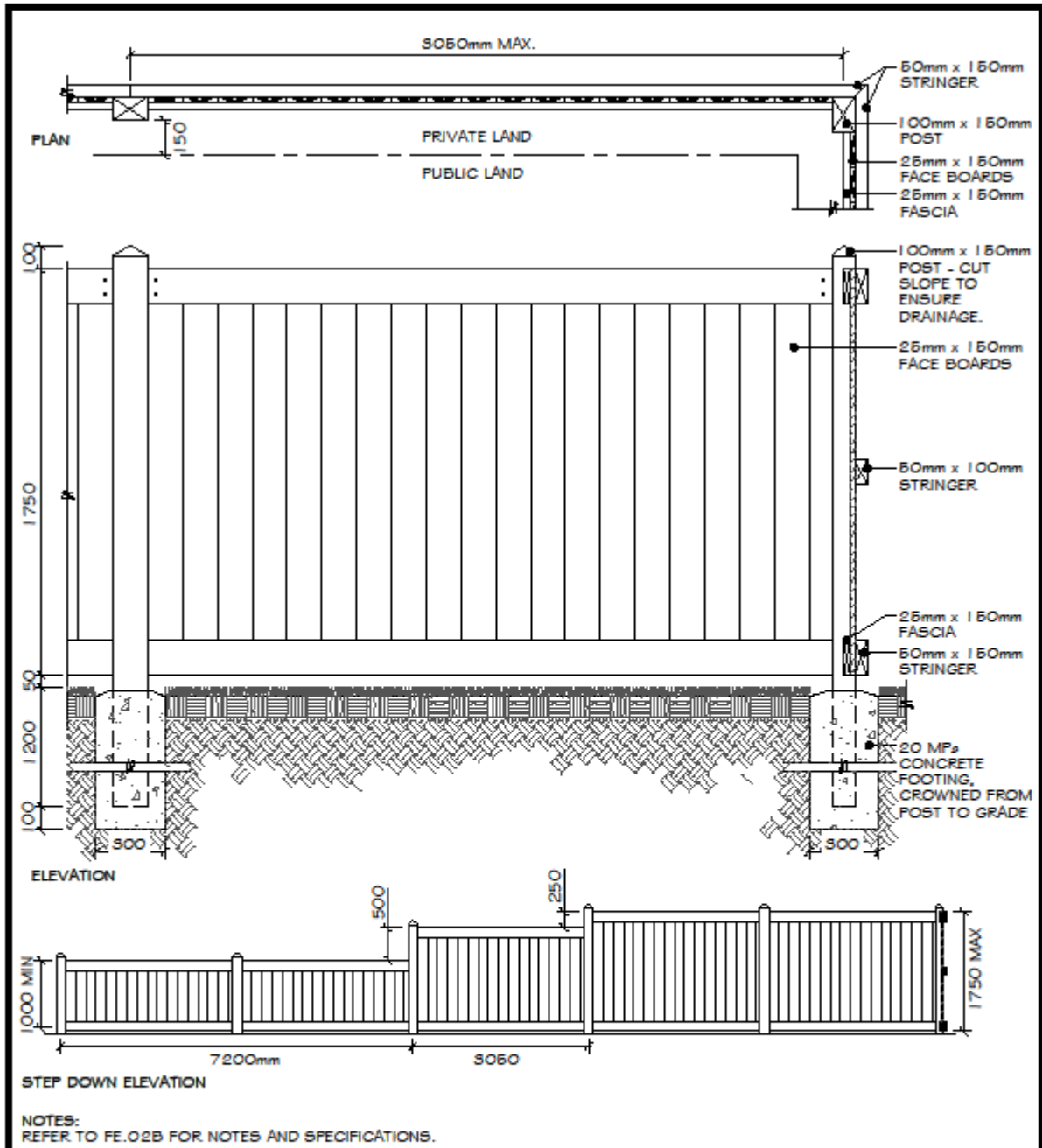
\_\_\_\_\_  
Date

**IF THIS FORM IS NOT RECEIVED PRIOR TO CONSTRUCTION, THEN THE PURCHASER WILL BE LIABLE FOR THE COST OF REPAIRS WHICH WILL BE DETERMINED AT THE TIME OF THE FINAL INSPECTION.**



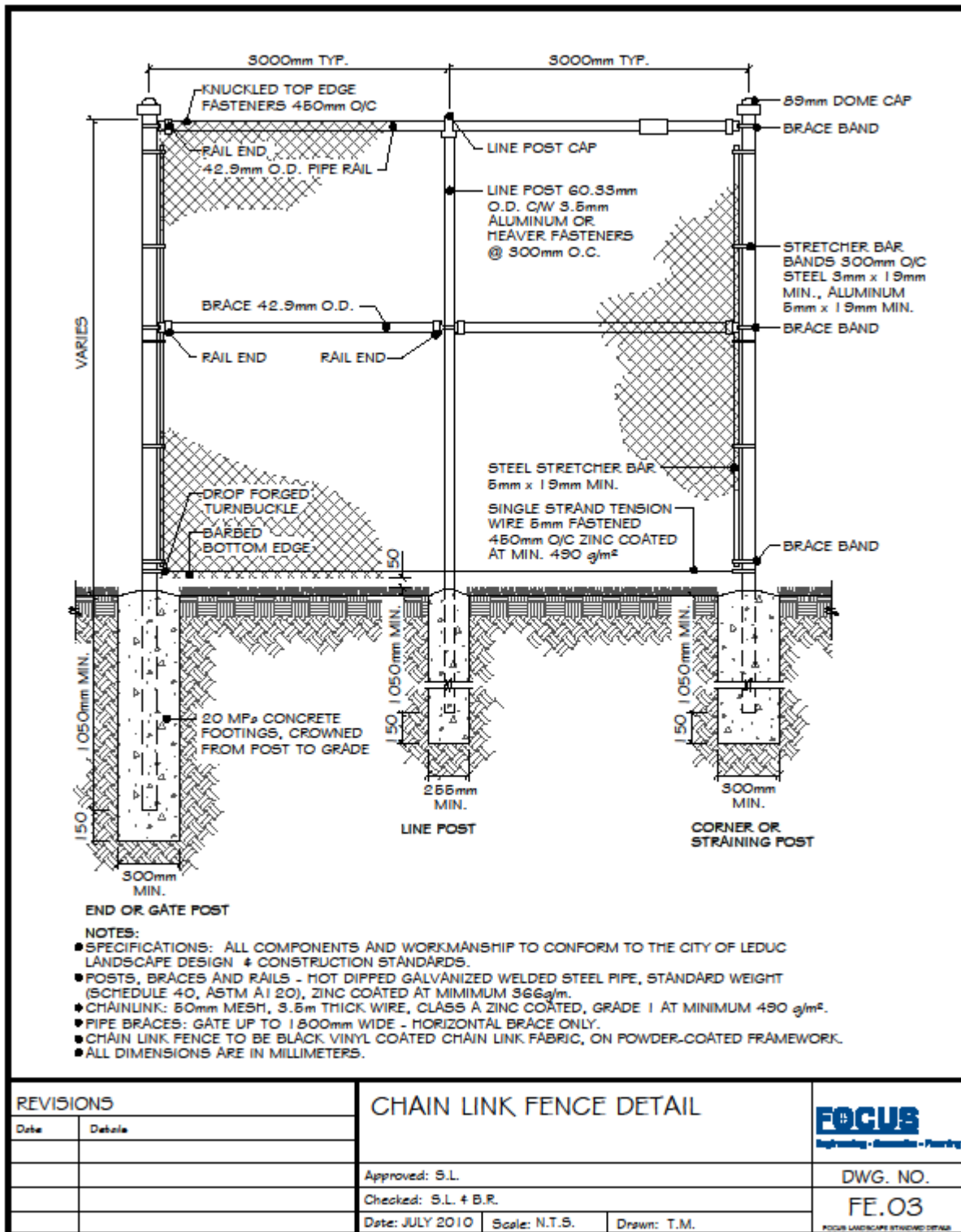
NOTES:  
REFER TO FE.02B FOR NOTES AND SPECIFICATIONS.

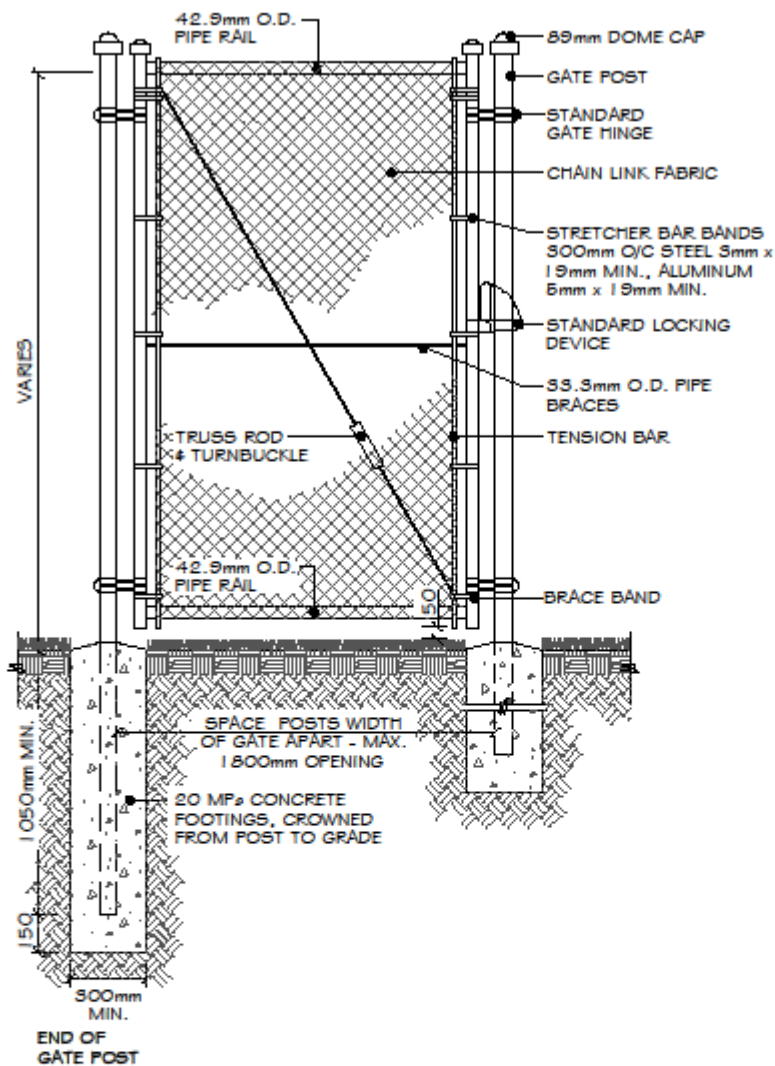
REVISIONS		WOOD SCREEN FENCE DETAIL		
Date	Details	Approved: S.L.	DWG. NO.	
		Checked: S.L. + B.R. <td>FE.02A</td> <td></td>	FE.02A	
		Date: JULY 2010	Scale: N.T.S.	Drawn: T.M.




N:\030100572 - Southfork 2010\landscape\0210\0207-2-1-300\drawings\construction\Drawings\Details for\Horton Design\FE.02A - Wood Screen Fence Detail.dwg

REVISIONS		WOOD SCREEN FENCE DETAIL		
Date	Details			
		Approved: S.L.		DWG. NO.
		Checked: S.L. & B.R.		FE.02A
		Date: JULY 2010	Scale: N.T.S.	Drawn: T.M.





- NOTES:**
- ◆ SPECIFICATIONS: ALL COMPONENTS AND WORKMANSHIP TO CONFORM TO THE CITY OF LEDUC LANDSCAPE DESIGN & CONSTRUCTION STANDARDS.
  - ◆ POSTS, BRACES AND RAILS - HOT DIPPED GALVANIZED WELDED STEEL PIPE, STANDARD WEIGHT (SCHEDULE 40, ASTM A120), ZINC COATED AT MINIMUM 566g/m.
  - ◆ CHAINLINK: 80mm MESH, 3.5m THICK WIRE, CLASS A ZINC COATED, GRADE 1 AT MINIMUM 490 g/m<sup>2</sup>.
  - ◆ PIPE BRACES: GATE UP TO 1800mm WIDE - HORIZONTAL BRACE ONLY.
  - ◆ CHAIN LINK FENCE GATE TO BE BLACK VINYL COATED CHAIN LINK FABRIC, ON POWDER-COATED FRAMEWORK.
  - ◆ ALL DIMENSIONS ARE IN MILLIMETERS.

REVISIONS		CHAIN LINK FENCE SINGLE GATE DETAIL	 Engineering • Consulting • Planning
Date	Details		
		Approved: S.L.	DWG. NO.
		Checked: S.L. & B.R.	FE.04
		Date: JULY 2010    Scale: N.T.S.    Drawn: T.M.	FOCUS LANDSCAPE ARCHITECTURE

N:\0301005752 - Southfork 2010\landscape\0301005752.dwg: Chain Link Single Gate Detail.dwg



